



ATTACHMENTS

GENERAL MEETING

**Wednesday 10 May 2023
at 6:30PM**



TABLE OF CONTENTS

OFFICE OF THE GENERAL MANAGER

1	GM17/23	Community Engagement Plan Housekeeping Amendments	
	Attachment 1:	HSC5963 Community Engagement Plan FINAL DRAFT April 2023	2

CORPORATE SUPPORT DIVISION

2	CS19/23	Investments and Borrowings for 2022/2023 - Status for Period Ending 31 March 2023	
	Attachment 1:	HSC Investments Summary Report March 2023.....	36
	Attachment 2:	HSC Borrowings Schedule March 2023	44
3	CS21/23	2022/23 Budget - March 2023 Quarter Review	
	Attachment 1:	Consolidated Budget Result	46
	Attachment 2:	Quarterly Budget Review Statement	47

PLANNING AND COMPLIANCE DIVISION

4	PC6/23	Report on Submissions - Housekeeping Development Control Plan Amendments	
	Attachment 1:	HDCP Housekeeping Amendments for Finalisation May 2023	60
5	PC8/23	Reporting Variations to Development Standards	
	Attachment 1:	Clause 4.6 Return - Jan to Mar 2023.....	93

ATTACHMENT/S

REPORT NO. PC6/23

ITEM 4

**1. HDCP HOUSEKEEPING AMENDMENTS FOR
FINALISATION MAY 2023**

Draft amendments to the Hornsby Development Control Plan 2013			
Section	Title	Page	Details of Amendment
1C.2.1	Transport and Parking	1-36	<ul style="list-style-type: none"> Under Note, amend reference from Section 2.118(2a) of <i>Transport and Infrastructure SEPP</i> to Section 2.119.
1C.2.5	Noise and Vibration	1-50	<ul style="list-style-type: none"> Under Note, amend reference from Section 2.119 of <i>Transport and Infrastructure SEPP</i> to Section 2.120.
2.1.3	Landscaping	2-8	<ul style="list-style-type: none"> Under Note, amend reference from Section 2.118(2a) of <i>Transport and Infrastructure SEPP</i> to Section 2.119.
2.2.7	Secondary Dwelling	2-16	<ul style="list-style-type: none"> Amend reference to HLEP Clause 5.4 to Clause 5.5. Amend prescriptive measure (a) to increase maximum total floor area from 60m² to 120m².
3.1.2	Setbacks	3-10	<ul style="list-style-type: none"> Amend Figure 3.1(d) to clarify side boundaries for battle-axe allotments.
4.1.1	Commercial Centres Hierarchy – Hornsby	4-5	<ul style="list-style-type: none"> Amend 4.1.1 heading to reference Hornsby LGA. Amend 'Major Centres' heading to 'Strategic Centres'. Replace 'Major Centres' paragraph with 'Strategic Centres' text from Hornsby ELS. Amend 'Town Centres' heading to 'Local Centres'. Replace 'Town Centres' paragraph with 'Local Centres' text from Hornsby ELS. Delete paragraph (d) referring to Epping Town Centre. Delete 'Stand Alone Shopping Centres' heading. Delete paragraph (e) referring to Carlingford. Amend 'Neighbourhood Centres' paragraph with 'Neighbourhood Centres' text from Hornsby ELS. Amend 'Villages and Small Villages' heading to 'Rural Village'. Amend 'Villages and Small Rural Villages' paragraph with 'Rural Villages' text from Hornsby ELS. Amend 'Enterprise Corridors' heading to 'Enterprise Corridors and Business Development Nodes'. Replace 'Enterprise Corridors' paragraph with 'Enterprise Corridors and Business Development Nodes' text from Hornsby ELS.
		4-6	<ul style="list-style-type: none"> Replace Figure 4.1(a) with hierarchy diagram from Hornsby ELS.
3.3.13	Vehicle Access and Parking	3-54	<ul style="list-style-type: none"> Under Note, amend reference from Section 2.118(2a) of <i>Transport and Infrastructure SEPP</i> to Section 2.119.
3.4.13	Vehicle Access and Parking	3-37	<ul style="list-style-type: none"> Under Note, amend reference from Section 2.118(2a) of <i>Transport and Infrastructure SEPP</i> to Section 2.119.
3.5.13	Vehicle Access and Parking	3-110	<ul style="list-style-type: none"> Under Note, amend reference from Section 2.118(2a) of <i>Transport and Infrastructure SEPP</i> to Section 2.119.
4.1.1	Commercial Centres Hierarchy – within the City of Parramatta LGA	4-5-a 4-6-a	<ul style="list-style-type: none"> Insert pages 4-5-a and 4-6-a for unchanged prescriptive measures and hierarchy diagram to continue applying for Parramatta LGA.
4.2.4	Landscaping	4-16	<ul style="list-style-type: none"> Under Note, amend reference from Section 2.118(2a) of <i>Transport and Infrastructure SEPP</i> to Section 2.119.

4.2.8	Vehicle Access and Parking	4-19	<ul style="list-style-type: none"> Under Note, amend reference from Section 2.118(2a) of <i>Transport and Infrastructure SEPP</i> to Section 2.119.
4.4.12	Vehicle Access and Parking	4-49	<ul style="list-style-type: none"> Under Note, amend reference from Section 2.118(2a) of <i>Transport and Infrastructure SEPP</i> to Section 2.119.
4.6.12	Vehicle Access and Parking	4-120	<ul style="list-style-type: none"> Under Note, amend reference from Section 2.118(2a) of <i>Transport and Infrastructure SEPP</i> to Section 2.119.
5.1.6	Vehicle Access and Parking	5-10	<ul style="list-style-type: none"> Under Note, amend reference from Section 2.118(2a) of <i>Transport and Infrastructure SEPP</i> to Section 2.119.
6.2.1	Residential Lands Subdivision	6-5	<ul style="list-style-type: none"> Amend 6.2.1 heading to reference Hornsby LGA.
		6-6	<ul style="list-style-type: none"> Amend paragraph (d) to include reference to measurement of lot width for battle-axe allotments. Amend Figure 6.2(b) to clarify that minimum lot width applies to battle-axe allotments. Amend Figure 6.2(b) caption to reference battle-axe allotments. Amend paragraph (f) to include reference to Figure 6.2(c).
		6-7	<ul style="list-style-type: none"> Amend Table 6.2(c) so that rear boundary setback is 3 metres. Amend Figure 6.2(d) to clarify side boundaries for battle-axe allotments.
6.2.1	Residential Lands Subdivision within the City of Parramatta LGA	6-5-a 6-6-a 6-7-a 6-8-a	<ul style="list-style-type: none"> Insert pages 6-5-a, 6-6-a, 6-7-a and 6-8-a for unchanged prescriptive measures to continue applying for the City of Parramatta LGA.
6.4.1	Residential and Rural Lands Accessway Design	6-13	<ul style="list-style-type: none"> Under Note, amend reference from Section 2.118(2a) of <i>Transport and Infrastructure SEPP</i> to Section 2.119.
9.6.14	Public Domain and Traffic Management Works	9-59	<ul style="list-style-type: none"> Under Note, amend reference from Section 2.118(2a) of <i>Transport and Infrastructure SEPP</i> to Section 2.119.
<i>Exhibited housekeeping amendments highlighted in blue</i>			
HLEP = Hornsby Local Environmental Plan 2013 HDCP = Hornsby Development Control Plan 2013 LGA = Local government area Hornsby ELS = Hornsby Employment Lands Study Transport and Infrastructure SEPP = State Environmental Planning Policy (Transport and Infrastructure) 2021			
HDCP amendments are indicated in the following pages: Deletions are in red text and strikethrough (i.e. delete) Additions are in green text (i.e. add)			

1C.2 Built Environment

The following section provides general controls for the protection of the built environment and applies to all forms of development. -

1C.2.1 Transport and Parking

Desired Outcomes

- Development that manages transport demand around transit nodes to encourage public transport usage.
- Car parking and bicycle facilities that meet the requirements of future occupants and their visitors.
- Development with simple, safe and direct vehicular access.

Prescriptive Measures

General

- Direct vehicular access to main roads should be avoided and/or access points consolidated.
- For development (other than single dwelling houses on existing lots), vehicle access and parking should be designed to allow vehicles to enter and exit the site in a forward direction.
- Design and dimensions of car parks, loading areas and driveways should comply with AS2890.1 and AS2890.2.
- Planning and design layout of parking areas for people with disabilities should be in accordance with AS2890.6 and AS1428.1.
- Planning and design layout of loading and manoeuvring areas should be provided in accordance with AS2890.2 and:
 - ¾ preferably be located to the side or rear of buildings,
 - ¾ screened from view from local and main roads, and
 - ¾ located so that vehicles do not stand on any public road, footway, laneway or service road.
- Planning and design layout of bicycle parking (rails, racks or lockers) should be designed in accordance with AS2890.3.

Dwelling Houses (additional general controls)

- The driveway to a single dwelling house should be located at least 6 metres from an intersection in accordance with AS2890.1.
- Driveways for single dwelling houses on existing lots should incorporate a dedicated turning area, designed to allow the 85% Design Car Turning Path, where:
 - ¾ there is poor sight distance from the driveway to pedestrian or vehicular traffic,
 - ¾ the accessway fronts a main road or highly pedestrianised area, or
 - ¾ where vehicles would otherwise have to reverse more than 50 metres.

- The minimum dimensions of car parking spaces for single dwelling houses should be in accordance with AS2890.1, as summarised in Table 1C.2.1(a):

Table 1C.2.1(a) Dwelling House - Parking Design Guide

Parking Type (residential)	Minimum Dimensions
Unobstructed parking space	2.4m(w) x 5.4m(l)
Single lock-up garage	3m(w) x 5.4m(l)
Double lock-up garage	5.7m(w) x 5.4m(l)

- The maximum grade for a driveway to a single dwelling house should be no greater than 25% with a maximum transition for changes of grade of 8% per plan metre. Table 1C.2.1(b) may be used as a guide in designing driveways.

Notes:

Main roads

Development adjoining roads that are subject to Section 2.118(2a) of the *State Environmental Planning Policy (Transport and Infrastructure) 2021* (Transport and Infrastructure SEPP) require separate approval from the RMS for access to State and Regional Roads as classified by the Roads and Maritime Services (RMS). A list of classified and unclassified main roads for Hornsby Shire as of September 2016 is provided in Annexure C.

Designated roads

Designated roads are Council identified roads that require development to have an increased setback from the road edge, consistent with the established streetscape. A list of designated roads is provided in Annexure C.

A highly pedestrianised area includes sites located in close proximity to schools, shopping centres, bus stops, places of worship and other busy community facilities.

Australian Standard AS2890 is available at www.sai-global.com.

** Design levels at the top of the adjacent kerb and gutter/crown or road must be obtained from Council's Works Division and the driveway design amended to comply with AS2890.1.

1C.2.5 Noise and Vibration

Desired Outcomes

- a. Development designed and managed to minimise noise and vibration impacts on the occupants of residential dwellings and other noise sensitive land uses.

Prescriptive Measures

Construction Noise Management

- a. Development proposals should be accompanied by documentation that includes a conceptual description of the measures to be applied to minimise construction noise.

Note:

Applicants should refer to the *Interim Construction Noise Guidelines (2009)* by the Department of Environment and Climate Change NSW available at www.environment.nsw.gov.au in preparing a noise management plan.

Noise Sensitive Development

- b. Noise sensitive landuses should include siting and design measures to ameliorate the potential impact of existing noise generating uses on the proposed development.
- c. Noise sensitive landuses adjoining a major road or a railway corridor should be accompanied by an acoustic report that demonstrates the site and building design is suitable for use in terms of acoustic amenity.
- d. High, solid acoustic fences should be avoided forward of the building line other than for noise sensitive landuses along major roads that are exposed to significant noise. In these instances, fences should be a maximum height of 1.8 metres and incorporate articulation. Large unbroken sections of fencing should be avoided.

Notes:

Noise sensitive landuses include dwellings or approved residential building envelopes on vacant lots, a place of public worship, a hospital, an educational establishment, a child care centre, a public open space area/park and other specialised commercial uses such as temporary accommodation (eg caravan parks or motels).

Major Roads for the purpose of this part of the DCP comprises roads with an annual average daily traffic volume of more than 40,000 vehicles, as defined by Section 2.419 2.120 of the Transport and Infrastructure SEPP that may include Pennant Hills Road, Beecroft Road, Epping Road, Castle Hill Road and Boundary Road.

Noise Generating Development

- e. Development should be sited and designed so that noise is kept to a minimum and does not create offensive noise as defined by the *Protection of the Environment Operations Act 1997*.
- f. Noise generating developments should be accompanied by an acoustic report that demonstrates the development is sited and designed to:
 - ¾ minimise the effect of noise and vibration on surrounding sensitive landuses, and
 - ¾ comply with relevant State Government and Council guidelines.
- g. The location and design of noise generating activities, such as loading and unloading areas, garbage collection areas, driveways, parking areas, active recreation areas, air conditioning or mechanical plants, should be sited away from adjacent sensitive landuses and/or screened by walls or other acoustic treatments.
- h. In addition to physical noise mitigation measures, noise impact management measures should be used to further limit potential noise impacts on sensitive landuses such as:
 - ¾ scheduled times to undertake noise generating activities and/or use of noise generating machinery, and
 - ¾ reasonable hours of operation including delivery hours.

Notes:

Noise generating development may include, but is not limited to the following: child care centres, schools, places of public worship, industrial uses, commercial developments, hotels, backpackers' accommodation, and some active recreational facilities.

For further information on relevant guidelines refer to:

- ¾ State Government Guidelines, including the *NSW Industrial Noise Policy* (EPA 2000) and the *NSW Environmental Criteria for Road Traffic Noise* (EPA 1999), available at www.environment.nsw.gov.au, and
- ¾ Transport and Infrastructure SEPP and the associated guidelines *Development Near Rail Corridors and Busy Roads - Interim Guideline* (DoP 2008) available at www.planning.nsw.gov.au, and
- ¾ Council's *Policy and Guidelines for Noise and Vibration Generating Development* available at website hornsby.nsw.gov.au.

2.1.3 Landscaping

Desired Outcomes

- a. Landscaping that integrates the built form with the locality and enhances the tree canopy.
- b. Landscaping that retains existing landscape features.
- c. Landscaping that is consistent with the visual landscapes in the rural area.

Prescriptive Measures

General

- a. Landscaping should maintain the natural features, topography and vegetation on the site.
- b. Setback areas should be landscaped.
- c. Vehicle crossings should be located to preserve natural vegetation which contributes to the visual amenity of the area.
- d. Intensive rural activities, should provide a landscape buffer to boundaries with a minimum width of 5 metres.

Retention of Landscape Features

- e. Buildings, driveways and service trenches should have a minimum setback:
 - ¾ in accordance with the 'Watercourses' element in Section 1C.1.3 of this DCP,
 - ¾ 10 to 20 metres to significant bushland as prescribed in the 'Biodiversity' element in Section 1C.1.1 of this DCP, and
 - ¾ in accordance with the requirements of AS4970 from significant trees to be retained.

Notes:

An intensive rural activity includes intensive agriculture, garden centres, plant nurseries and landscaping material supplies, animal boarding or training establishments, rural industries, extractive industries and the like.

A Landscape buffer is to include screen planting, preferably including vegetation that is endemic to the area. Alternatively, fire retardant species should be considered in bushfire prone areas.

The applicant is encouraged to incorporate species from Council's publication *Indigenous Plants for the Bushland Shire* available on Council's website hornsby.nsw.gov.au.

Main roads

Development adjoining roads that are subject to Section 2.118(2a) of the Transport and Infrastructure SEPP require separate approval from the RMS for access to State and Regional Roads as classified by the Roads and Maritime Services (RMS). A list of classified and unclassified main roads for Hornsby Shire as of September 2016 is provided in Annexure C.

Fences and Gates

- f. Frontages/ streetscapes should not contain excessively urban features such as formal gates and high fences.
- g. Fences should be open style and constructed of materials such as timber or post and wire, with a maximum height of 1.8 metres.
- h. Any masonry gate entry feature should not extend more than 3 metres either side of the driveway entrance.
- i. High, solid fences constructed as sound barriers should be avoided. On main roads alternative measures of reducing traffic noise should be explored, such as double glazing, internal layout, earth mounds and vegetation, rather than high solid fences.



Figure 2.1(a) Example of a suitable open style rural fence. (E)



Figure 2.1(b) Example of a masonry entry feature that does not extend more than 3 metres either side of the driveway. (E)

2.2.7 Secondary Dwelling

HLEP Clause 5.45 contains provisions for development of Secondary Dwellings on rural land. The following controls apply to land zoned RU1 Primary Production, RU2 Rural Landscape, RU4 Small Lot Primary Production, and RU5 Village.

Desired Outcomes

- a. Secondary dwellings that provide opportunity for an extension of family accommodation or affordable rental accommodation in the rural area and maintain the rural character of the area.

Prescriptive Measures

- a. A secondary dwelling should:
 - have a maximum total floor area of 60 120m², or
 - 33% of the total floor area of the principal dwelling, whichever is the greater.
- b. Secondary dwellings should not be attached to farm buildings or rural buildings.
- c. Secondary dwellings should be located not to affect the capability of land to be used for agriculture.
- d. Vehicular access to both dwellings should be from a single common driveway or access from the public road.
- e. Where the creation of a secondary dwelling involves an extension to an existing dwelling house, the secondary dwelling should be constructed of the same materials of the existing dwelling, or the existing dwelling should be renovated to match the proposed external materials of the new dwelling.
- f. Where the primary and secondary dwelling are attached on land zoned RU1, RU2 and RU4, the total length of the front elevation should not exceed 50% of the frontage of the lot.
- g. Subdivision of land to provide a separate lot for an approved secondary dwelling is not supported if the resultant lots are smaller than the applicable minimum lot size shown on the HLEP Lot Size Map, pursuant to Clause 2.6 of the HLEP.
- h. A secondary dwelling can not be erected on a separate lot created for the purposes of primary production pursuant to Clause 4.2 of the HLEP.

Notes:

A secondary dwelling means a self-contained dwelling that:

- (a) is established in conjunction with another dwelling (the principal dwelling), and
- (b) is on the same lot of land as the principal dwelling, and
- (c) is located within, or is attached to, or is separate from, the principal dwelling.

Total Floor Area means gross floor area as defined by the HLEP.

Case Study Example:

Mike and Lisa own a 2 hectare block of land zoned RU4 and have developed a 500m² principal dwelling on the site (excluding the garage). The property can therefore accommodate a secondary dwelling with a gross floor area of up to 165m² (33% of 500m²).

In determining where and how the secondary dwelling should be accommodated, they now need to consider the provisions of 2.2.7 of the DCP and other applicable controls, (eg. Part 1 and Section 2.1 of the DCP) that includes controls such as:

- Scale (height, roof design, design details)
- Setbacks (including separation to intensive rural activities)
- Private Open Space,
- Car parking,
- Biodiversity,
- Bushfire,
- Landscaping,
- Effluent Disposal, and
- Building Sustainability (BASIX)

Floor area of a dwelling house (as defined by the NSW Housing Code) means the sum of the areas of each storey of the dwelling house and any carport, garage, balcony, deck, patio, pergola, terrace or verandah, measured at a height of 1.4m above each floor level, that is within the outer face of:

- (a) the external walls of the dwelling house, and
 - (b) the walls of the carport, garage, balcony, deck, patio, pergola, terrace or verandah,
- but does not include any of the following:
- (c) any part of an awning, blind or canopy that is outside the outer wall of a building,
 - (d) the eaves,
 - (e) a lift shaft,
 - (f) a stairway,
 - (g) a void above a lower storey.

Outbuilding (as defined by the NSW Housing Code) means any of the following:

- (a) balcony, deck, patio, pergola, terrace or verandah that is detached from a dwelling house,
- (b) cabana, cubby house, fernery, garden shed, gazebo or greenhouse,
- (c) carport that is detached from a dwelling house,
- (d) farm building,
- (e) garage that is detached from a dwelling house,
- (f) rainwater tank (above ground) that is detached from a dwelling house,
- (g) shade structure that is detached from a dwelling house,
- (h) shed.

3.1.2 Setbacks

Desired Outcome

- a. Setbacks that are compatible with adjacent development and complement the streetscape.
- b. Setbacks that allow for canopy trees to be retained and planted along the front and rear property boundaries.

Prescriptive Measures

- a. The minimum setback of all buildings and structures to the boundaries of the site should comply with Table 3.1.2(a):

Table 3.1.2(a): Minimum Boundary Setbacks

Boundary Setback	Minimum Building Setback
Front Boundary (primary frontage)	6m to local roads and 9m to designated roads, except for the following: <ul style="list-style-type: none"> ■ On local roads, where an existing setback of 7.6m or greater exists, it may be necessary to conform to this setback to maintain the streetscape character, and ■ 3m to Brooklyn Road, Brooklyn, and ■ 9m to roads in Cherrybrook
Waterfront Setback	See Clause 6.1 of HLEP Foreshore Building Line Map
Secondary Boundary (on corner lots)	3m
Side Boundary	up to 1 storey = 0.9m 2 storey element = 1.5m
Rear Boundary	up to 1 storey = 3m 2 storey element = 8m

- b. For the purpose of the setback controls, a 1 storey building or element is not to exceed a building height of 4.5 metres above existing ground level.
- c. For buildings with a corner frontage, front and rear boundary setbacks apply to the shorter street frontage as illustrated in Figure 3.1(c).
- d. For the purpose of calculating setbacks for a battle-axe lot, the setback on the opposite side of the lot to the rear setback is taken to be a side setback, as illustrated in Figure 3.1 (d).
- e. For a lot that has boundaries with parallel roads, the front boundary setback control applies to both property boundaries.

- f. Notwithstanding the above, the minimum side boundary setback of a tennis court should be 3 metres to provide for screen planting.
- g. The setback of the dwelling and ancillary structures from the property boundary may need to be increased to maintain landscape features, as detailed in Section 3.1.3 of this DCP.

Permissible Encroachments into Building Setbacks

- h. On local roads, where the streetscape will not be adversely affected, a single storey encroachment of 1.5 metres may be permitted for a distance equal to 1/3 of the width of the dwelling measured at the building line. Any encroachment is not to be in the form of a garage.
- i. The following minor structures are able to encroach into the prescribed setbacks:
- A driveway between the on-site car parking area and a public road,
 - Stairs to the ground floor of the dwelling,
 - Fences,
 - A single storey outbuilding, with a maximum floor area of 25m², is able to encroach to within 0.9 metres of the rear boundary (eg. garden shed, garage, pergola), and
 - An inground swimming pool is able to encroach to within 1 metre of the rear boundary, measured to the water line.

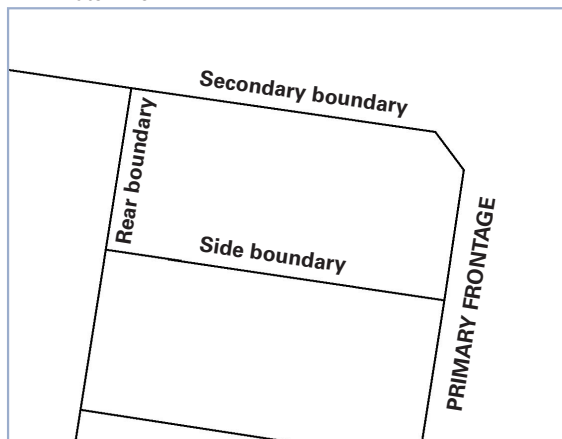


Figure 3.1(c): Setbacks on corner lots.(l)

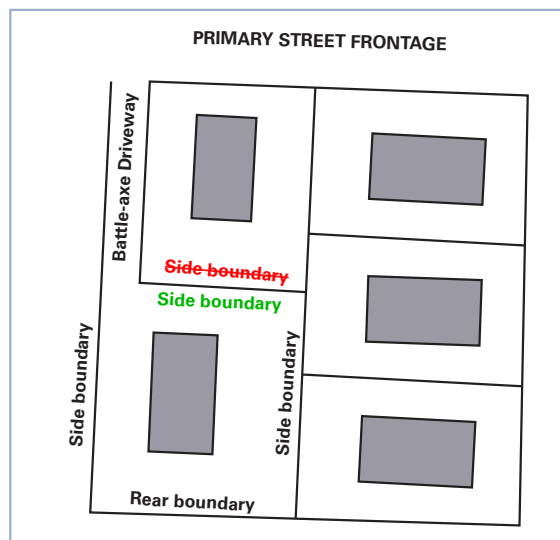


Figure 3.1(d): Setbacks on battle-axe lots.(l)

Notes:

The rear boundary is ordinarily located parallel to and/or opposite the primary frontage.

Designated roads

Designated roads are Council identified roads that require development to have an increased setback from the road edge, consistent with the established streetscape. A list of designated roads is provided in Annexure C.

3.3.13 Vehicle Access and Parking**Desired Outcome**

- a. Development that provides sufficient and convenient parking for residents and visitors with vehicular access that is simple, safe and direct.

Prescriptive Measures**General**

- a. Direct access to main roads should be avoided.
- b. Driveways should be located at least 2 metres from any side boundary and flanked by continuous landscaped verges.
- c. Resident and visitor parking should be provided within basements.
- d. Any undercroft carparking should be screened and not be located in a dwelling facade that faces a primary or secondary street frontage.
- e. Driveways and garage entrances should not visually dominate any street or facade that facades a communal area upon the site.
- f. Parking for service and delivery vehicles should be integrated with the design of driveways and landscaped verges and not visually dominate any street frontage.

Ancillary Fixtures and Facilities

- g. Separate dedicated and secure storage areas for each dwelling should be provided in basement car parks suitable to accommodate larger items such as sporting equipment.

Note:

Refer to Part 1 'General' of the DCP for car parking and bicycle parking rates and ancillary general design requirements.

Main roads

Development adjoining roads that are subject to Section 2.118(2a) 2.119 of the Transport and Infrastructure SEPP require separate approval from the RMS for access to State and Regional Roads as classified by the Roads and Maritime Services (RMS). A list of classified and unclassified main roads for Hornsby Shire as of September 2016 is provided in Annexure C.

3.3.14 Public Domain and Traffic Management Works**Desired Outcome**

- a. A public domain that encourages vitality around and within development precincts.
- b. Traffic management works that provide for the safe and efficient movement of vehicles to, from and within precincts.

Prescriptive Measures**Public Domain**

- a. Development of the public domain should make the locality an attractive place that encourages development and provides amenity for residents.
- b. Embellishment of the public domain should include street furniture, new street plantings and footpath improvements and other work in accordance with the Epping Town Centre Public Domain Guideline.
- c. Pedestrian linkages shown on the Key Development Principles Diagrams and Town Centre Linkage Diagrams (Annexure B) should be provided and reinforced as safe, accessible and vibrant pedestrian areas.

Traffic Management Works

- d. Traffic management works should be undertaken in accordance with the traffic improvements identified in the Key Development Principles Diagrams.
- e. Council or the relevant authority will undertake the necessary traffic management improvements located on public land and roads. Development should be designed to accommodate and complement the proposed traffic improvements or offer alternative traffic management solutions.
- f. Development proposing alternative traffic management solutions should be accompanied by a comprehensive traffic assessment.

Note:

This DCP will inform Council's Civic Works Program and Street Tree Planting Program.

For development within Epping Town Centre, refer to the Epping Town Centre Public Domain Guideline available at [hornsby.nsw.gov.au](https://www.hornsby.nsw.gov.au)

3.4.13 Vehicle Access and Parking

Desired Outcome

- a. Development that provides sufficient and convenient parking for residents and visitors with vehicular access that is simple, safe and direct.

Prescriptive Measures

General

- a. Direct access to main roads should be avoided.
- b. Driveways should be located at least 2 metres from any side boundary and flanked by continuous landscaped verges.
- c. Resident and visitor parking should be provided within basements.
- d. Any undercroft carparking should be screened and should not be located in a dwelling facade that faces a primary or secondary street frontage.
- e. All ramps are to be designed as two way ramps in accordance with AS 2890.1 and AS 2890.2
- f. All ramps are to be designed in accordance with the exits and entry widths of AS 2890.1 and AS 2890.2
- g. Driveways and garage entrances should not visually dominate any street or facade that facades a communal area upon the site.
- h. Parking for service and delivery vehicles should be integrated with the design of driveways and landscaped verges and should not visually dominate any street frontage.

Ancillary Fixtures and Facilities

- i. Separate dedicated and secure storage areas for each dwelling should be provided in basement car parks suitable to accommodate larger items such as sporting equipment.

Note:

Refer to Part 1 'General' of the DCP for car parking and bicycle parking rates and ancillary general design requirements.

Main roads

Development adjoining roads that are subject to Section 2.418(2a) 2.119 of the Transport and Infrastructure SEPP require separate approval from the RMS for access to State and Regional Roads as classified by the Roads and Maritime Services (RMS). A list of classified and unclassified main roads for Hornsby Shire as of September 2016 is provided in Annexure C.

3.4.14 Public Domain and Traffic Management Works

Desired Outcomes

- a. A public domain that encourages vitality around and within development precincts.
- b. Traffic management works that provide for the safe and efficient movement of vehicles to, from and within precincts.

Prescriptive Measures

Public Domain

- a. Development of the public domain should make each precinct an attractive place that encourages development and provides amenity for residents.
- b. Embellishment of the public domain should include street furniture, new street plantings, and footpath improvements.
- c. Pedestrian linkages shown on the Key Development Principles Diagrams and Town Centre Linkage Diagrams (Annexure B) should be provided and reinforced as safe, accessible and vibrant pedestrian areas.

Traffic Management Works

- d. Traffic management works should be undertaken in accordance with the traffic improvements identified in the Key Development Principles Diagrams, and Traffic Management Improvement Plans Figures 3.4(j), 3.4(k) and 3.4(l).
- e. Council or the relevant authority will undertake the necessary traffic management improvements located on public land and roads. Development should be designed to accommodate and complement the proposed traffic improvements or offer alternative traffic management solutions.
- f. Development proposing alternative traffic management solutions should be accompanied by a comprehensive traffic assessment.

Note:

This DCP will inform Council's Civic Works Program and Street Tree Planting Program.

For development within Epping Town Centre, refer to the Epping Town Centre Public Domain Guidelines available at [hornsby.nsw.gov.au](https://www.hornsby.nsw.gov.au)

3.5.13 Vehicle Access and Parking

Desired Outcome

- a. Development that provides sufficient and convenient parking for residents and visitors with vehicular access that is simple, safe and direct.

Prescriptive Measures

General

- a. Direct access to main roads should be avoided.
- b. Driveways should be located at least 2 metres from any side boundary and flanked by continuous landscaped verges. (*excluding Pound Road, Hornsby Precinct*).
- c. In the Pound Road, Hornsby precinct, vehicular access should be provided via the accessway (Wanderers Way) at the rear of the precinct.
- d. Resident and visitor parking should be provided within basements.
- e. All ramps are to be designed as two way ramps in accordance with AS 2890.1 and AS 2890.2
- f. All ramps are to be designed in accordance with the exits and entry widths of AS 2890.1 and AS 2890.2
- g. Any undercroft car parking should be screened and should not be located in a dwelling facade that faces a primary or secondary street frontage.
- h. Driveways and garage entrances should not visually dominate any street or facade that faces a communal area upon the site.
- i. Parking for service and delivery vehicles should be integrated with the design of driveways and surrounding landscaped verges, and should not visually dominate any street frontage.

Ancillary Fixtures and Facilities

- j. Separate dedicated and secure storage areas for each dwelling should be provided in basement car parks suitable to accommodate larger items such as sporting equipment.

Note:

Refer to Part 1 'General' of the DCP for car parking and bicycle parking rates and ancillary general design requirements.

Main roads

Development adjoining roads that are subject to Section 2.118(2a) 2.119 of the Transport and Infrastructure SEPP require separate approval from the RMS for access to State and Regional Roads as classified by the Roads and Maritime Services (RMS). A list of classified and unclassified main roads for Hornsby Shire as of September 2016 is provided in Annexure C.

4.1 Commercial Centres Hierarchy

4.1.1 Commercial Centres Hierarchy - within Hornsby LGA

Desired Outcome

- a. Development that reinforces the role and function of the centre in the commercial centres hierarchy.

Prescriptive Measures

- a. Development should reinforce the commercial centre hierarchy identified at Figure 4.1(a) and described in the following:

Major Strategic Centres

- b. ~~Hornsby Town Centre is a Major Centre serving the North Subregion. This area should provide much of the civic, cultural, retail and economic requirements for the Subregion. The centre should develop additional employment generating activities including offices and services.~~
Hornsby Town Centre is a Strategic Centre serving the North District. This centre should contribute to the civic, cultural, retail and economic requirements for the District. The centre should accommodate a diversity of employment opportunities and be the primary location for offices and services.

Town Local Centres

- c. ~~Town Centres should provide a wide range of goods and services to the community. Trips to larger centres such as Hornsby Town Centre should only be required for higher order commodities.~~

Local Centres should provide a wide range of goods and services, including a supermarket, for the community. Trips to larger centres such as Hornsby Town Centre should only be required for higher order commodities. They typically contain a supermarket over 1,000m².

- d. ~~While Epping has a Town Centre status within the Metropolitan Strategy, the component of the centre within Hornsby Shire operates predominately as a Village and this role should be maintained.~~

Stand Alone Shopping Centres

- e. ~~Carlingford is a Stand Alone Shopping Centre that is internalised and located away from other commercial areas. It contains many of the attributes of a Town Centre and may have the potential to become a traditional Town Centre in the long term.~~

Neighbourhood Centres

- g. ~~Neighbourhood Centres provide retail and other services to the immediately surrounding residential area. The role of Neighbourhood Centres to serve the needs of local residents should be reinforced. To ensure that Neighbourhood Centres do not undermine the strength of the commercial environment of surrounding larger centres, commercial/retail uses that serve the wider community should not be located within Neighbourhood Centres.~~

- d. Neighbourhood Centres provide a range of small-scale retail and other services that serve the convenience needs of people that live and work in the surrounding neighbourhood. Higher order retail and commercial uses that serve the wider community are not located in neighbourhood centres.

Villages and Small Rural Villages

- f. ~~The smaller local centres known as Villages and Small Villages are identified in the commercial centres hierarchy. These centres should be the preferred location for small and medium scale commercial/retail uses that serve the local community and only have limited office and bulky good retail functions.~~

- e. Rural villages provide retail, commercial and employment opportunities for their local community. They typically provide under 2,000m² of retail space, may contain a small neighbourhood supermarket (under 1,000m²) and are zoned RU5 - Village.

Enterprise Corridors and Business Development Nodes

- h. ~~Parts of Pennant Hills Road and the Pacific Highway function as Enterprise Corridors providing accommodation for local and regional services that benefit from high levels of passing traffic such as start-up offices, light industry, motor showrooms, building supplies and bulky good retail.~~

- f. Enterprise Corridors and Business Development Nodes provide accommodation for local and district services that benefit from high levels of passing traffic such as start-up offices, light industry, motor showrooms, building supplies and bulky good retail. They provide essential population support services that meet the day to day needs of their surrounding community. They support the function of local centres.

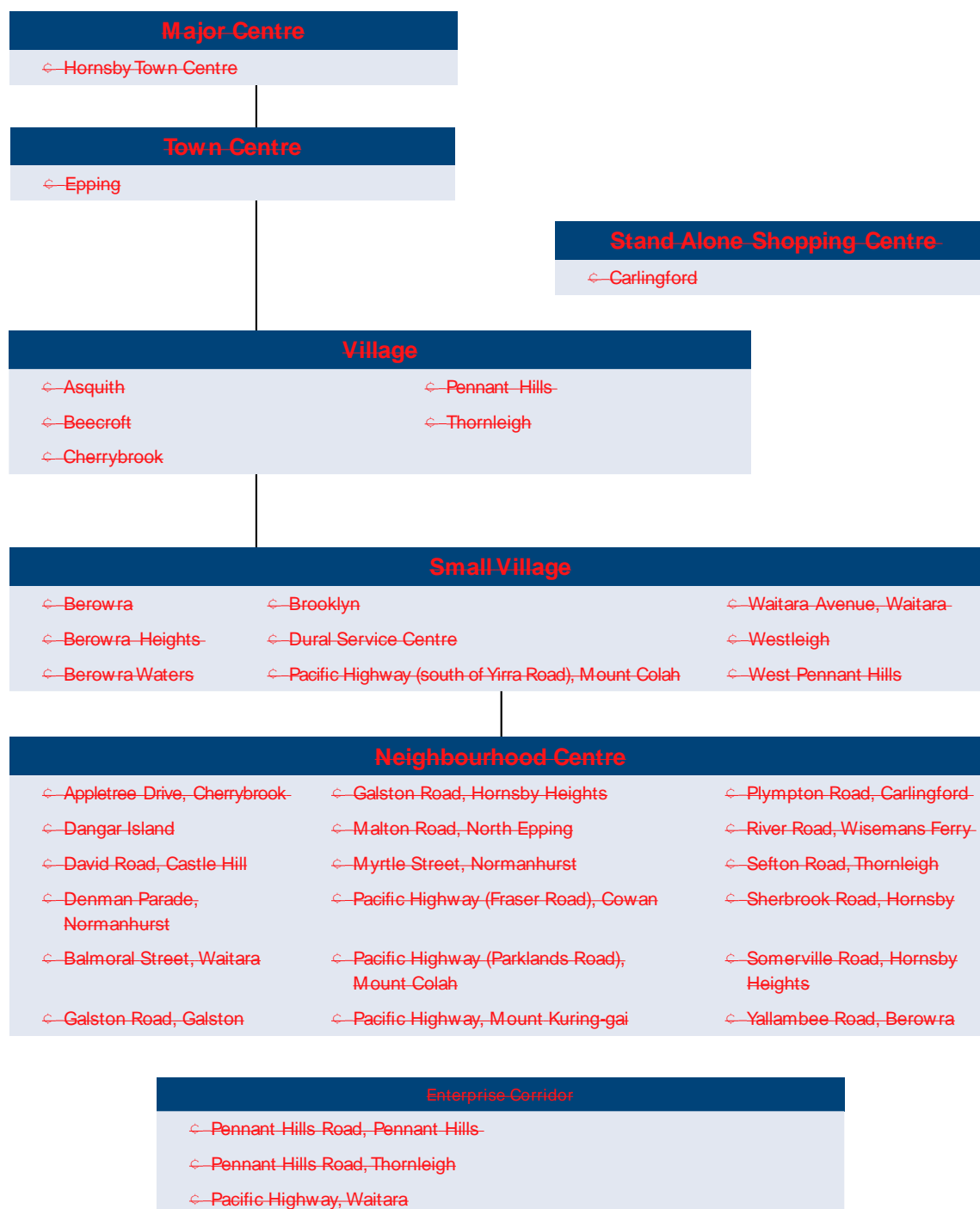


Figure 4.1(a): Commercial Centres Hierarchy.(C)

Strategic Centre		
Hornsby Town Centre		
Local Centres		
Thornleigh Village	Asquith Village	Galston Road Village
Cherrybrook Village	West Pennant Hills Village	Westleigh Village
Pennant Hills Village	Berowra Village	Pacific Highway Mount Kuring-Gai
Berowra Heights Village	Dural Service Centre	Waitara Village
Beecroft Village		
Neighbourhood Centre		
Appletree Drive, Cherrybrook	Galston Road, Hornsby Heights	Pacific Highway, Cowan
Dangar Island	Malton Road, North Epping	Parklands Road, Mount Colah
David Road, Castle Hill	Myrtle Street, Normanhurst	Wisemans Ferry
Denman Parade, Normanhurst	Mount Colah Village	Sefton Road, Thornleigh
Balmoral Street, Waitara		Yallambee Road, Berowra
Brooklyn Village		
Rural Village		
Dural Rural Village		
Enterprise Corridor and Business Development Nodes		
Pennant Hills Road, Pennant Hills		
Pennant Hills Road, Thornleigh		
Pacific Highway, Waitara		

Figure 4.1(a): Commercial Centres Hierarchy.(C)

4.1 Commercial Centres Hierarchy

4.1.1 Commercial Centres Hierarchy - Hornsby within City of Parramatta LGA

Desired Outcome

- a. Development that reinforces the role and function of the centre in the commercial centres hierarchy.

Prescriptive Measures

- a. Development should reinforce the commercial centre hierarchy identified at Figure 4.1(a) and described in the following:

Major Centres

- b. Hornsby Town Centre is a Major Centre serving the North Subregion. This area should provide much of the civic, cultural, retail and economic requirements for the Subregion. The centre should develop additional employment generating activities including offices and services.

Town Centres

- c. Town Centres should provide a wide range of goods and services to the community. Trips to larger centres such as Hornsby Town Centre should only be required for higher order commodities.
- d. While Epping has a Town Centre status within the Metropolitan Strategy, the component of the centre within Hornsby Shire operates predominately as a Village and this role should be maintained.

Stand Alone Shopping Centres

- e. Carlingford is a Stand Alone Shopping Centre that is internalised and located away from other commercial areas. It contains many of the attributes of a Town Centre and may have the potential to become a traditional Town Centre in the long-term.

Villages and Small Villages

- f. The smaller local centres known as Villages and Small Villages are identified in the commercial centres hierarchy. These centres should be the preferred location for small and medium scale commercial/retail uses that serve the local community and only have limited office and bulky good retail functions.

Neighbourhood Centres

- g. Neighbourhood Centres provide retail and other services to the immediately surrounding residential area. The role of Neighbourhood Centres to serve the needs of local residents should be reinforced. To ensure that Neighbourhood Centres do not undermine the strength of the commercial environment of surrounding larger centres, commercial/retail uses that serve the wider community should not be located within Neighbourhood Centres.

Enterprise Corridors

- h. Parts of Pennant Hills Road and the Pacific Highway function as Enterprise Corridors providing accommodation for local and regional services that benefit from high levels of passing traffic such as start-up offices, light industry, motor showrooms, building supplies and bulky good retail.

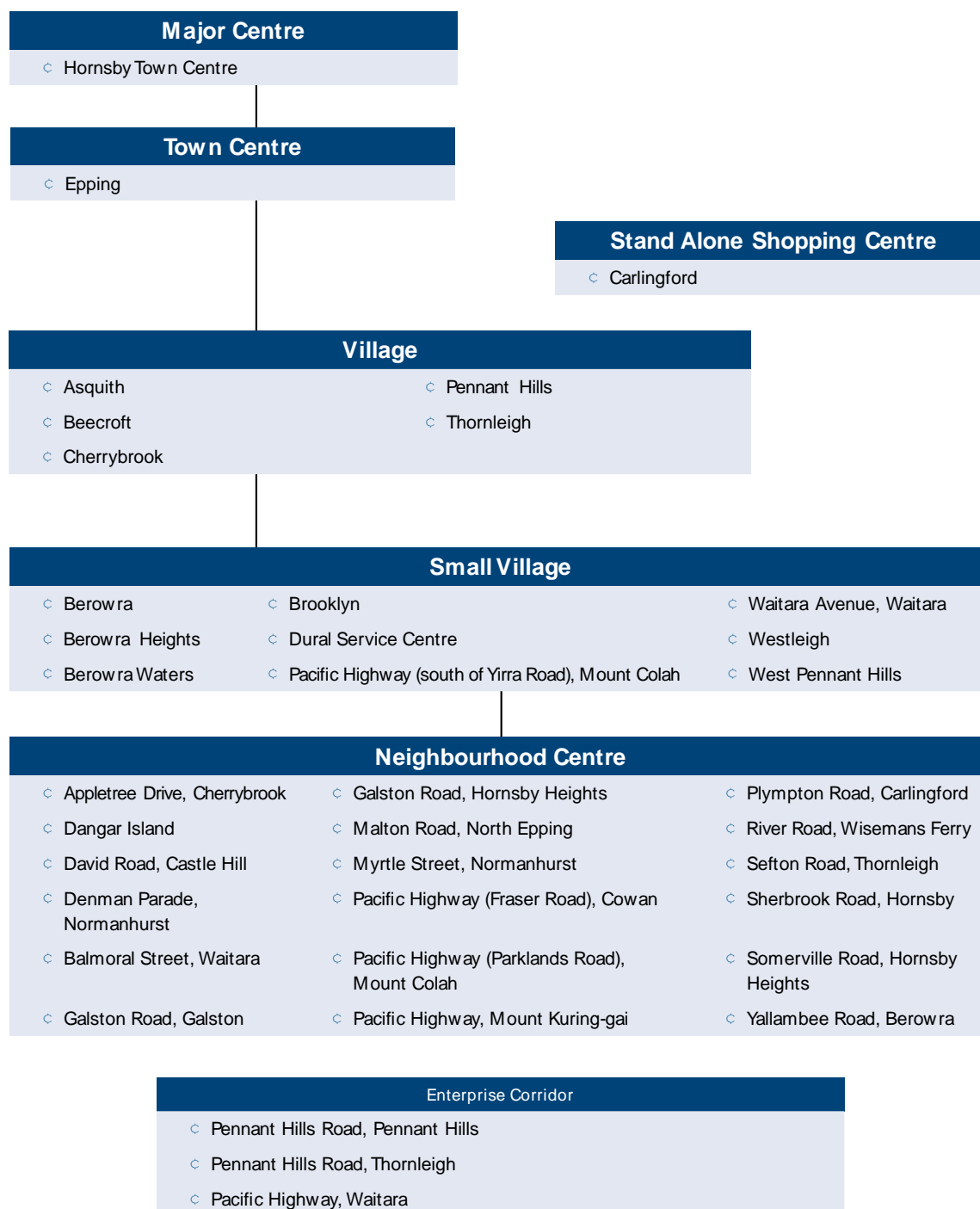


Figure 4.1(a): Commercial Centres Hierarchy.(C)

4.2.4 Landscaping

Desired Outcome

- a. Development that contributes to attractive streetscapes by providing shade along pedestrian frontages and screen planting along boundaries.
- b. Development that preserves significant trees that add to the environmental character of the commercial centre.

Prescriptive Measures

General

- a. Landscaping should be included in building setback areas to complement the appearance of the building.
- b. Setbacks from sensitive areas should be fully landscaped.
- c. Primary and secondary retail frontages should be landscaped with tree-plantings combined with paving in accordance with the following:
 - ¾ Street tree planting should be provided where appropriate having regard to site lines, footpath widths, underground services and awnings. Consideration should be given to the use of trees to provide shade in summer and allow sunlight in winter when selecting and positioning trees.
 - ¾ Pavements within each precinct should be of a consistent design, constructed of durable and non-slip modular units that are resistant to fading, discolouration and chipping, and that may readily be removed and replaced following future installation of in-ground services.
- d. Landscaping along Old Northern Road and New Line Roads should incorporate grass swales and dense vegetation planting.

Shop Top Housing

- e. Residential levels should be landscaped with native or exotic species in planter boxes watered by recycled grey water or stormwater to provide screening.
- f. Where communal open space is required, these spaces should include lawn areas surrounded by hedges of shrubs.

Retention of Landscape Features

- g. The proposed building, ancillary structures, driveways, drainage and service trenches should be setback:
 - ¾ in accordance with the 'Watercourses' element in Section 1C.1.3 of this DCP,
 - ¾ 10-20 metres to significant bushland as detailed in the 'Biodiversity' element in Section 1C.1.1 of this DCP, and
 - ¾ in accordance with the requirements of AS 4970 for significant trees to be retained.

Fencing

- h. Fencing is discouraged in the primary and secondary boundary setbacks.
- i. Allotments adjoining residential lands should be fenced with appropriate residential style fencing.
- j. Fencing enclosing private residential courtyards may be up to 1.8 metres high if constructed from lightweight materials with the design allowing at least 50 percent openings/ transparency.
- k. Fencing associated with development in the Dural Service Centre should not be provided within the setback areas of main or local roads.

Notes:

Sensitive areas include any adjoining residential lands, community uses, educational uses, public open spaces and recreational areas.

The applicant is encouraged to incorporate species from Council's publication *Indigenous Plants for the Bushland Shire* available at hornsby.nsw.gov.au as part of the development.

Main roads

Development adjoining roads that are subject to Section 2.118(2a) 2.119 of the Transport and Infrastructure SEPP require separate approval from the RMS for access to State and Regional Roads as classified by the Roads and Maritime Services (RMS). A list of classified and unclassified main roads for Hornsby Shire as of September 2016 is provided in Annexure C.

4.2.8 Vehicle Access and Parking

Desired Outcome

- a. Development that provides sufficient and convenient parking for residents and visitors with vehicular access that is simple, safe and direct.

Prescriptive Measures

Vehicular Access

- a. Access to garages and storage areas should be confined to side and rear facades, with access from main roads avoided.
- b. For development in the Dural Service Centre, vehicular access to New Line Road should be via service lanes and vehicular access to Old Northern Road should be via service roads, in accordance with the Traffic Management Strategy as discussed at Section 4.2.9.

Note:

Refer to Part 1 'General' of the DCP for car parking, service vehicle, bicycle parking provisions and ancillary general design requirements.

Main roads

Development adjoining roads that are subject to Section 2.118(2a) 2.119 of the Transport and Infrastructure SEPP require separate approval from the RMS for access to State and Regional Roads as classified by the Roads and Maritime Services (RMS). A list of classified and unclassified main roads for Hornsby Shire as of September 2016 is provided in Annexure C.

Parking

c. On-site car parking should:

- ¾ be provided behind buildings or beneath buildings in a basement,
- ¾ not be sited within a front setback area,
- ¾ be accessed via rear laneways or side streets where available,
- ¾ be screened from the street and other public areas by landscaping,
- ¾ design the basement car park entrance to incorporate other facade elements such as overhanging balconies or side planter boxes in the composition of the facade,
- ¾ All ramps are to be designed as two way ramps accordance with AS 2890.1 and AS 2890.2, and
- ¾ All ramps are to be designed in accordance with the exits and entry widths of AS 2890.1 and AS 2890.2.

Ancillary Fixtures and Facilities

- d. Separate dedicated and secure storage areas for each dwelling should be provided in basement car parks suitable to accommodate larger items such as sporting equipment.

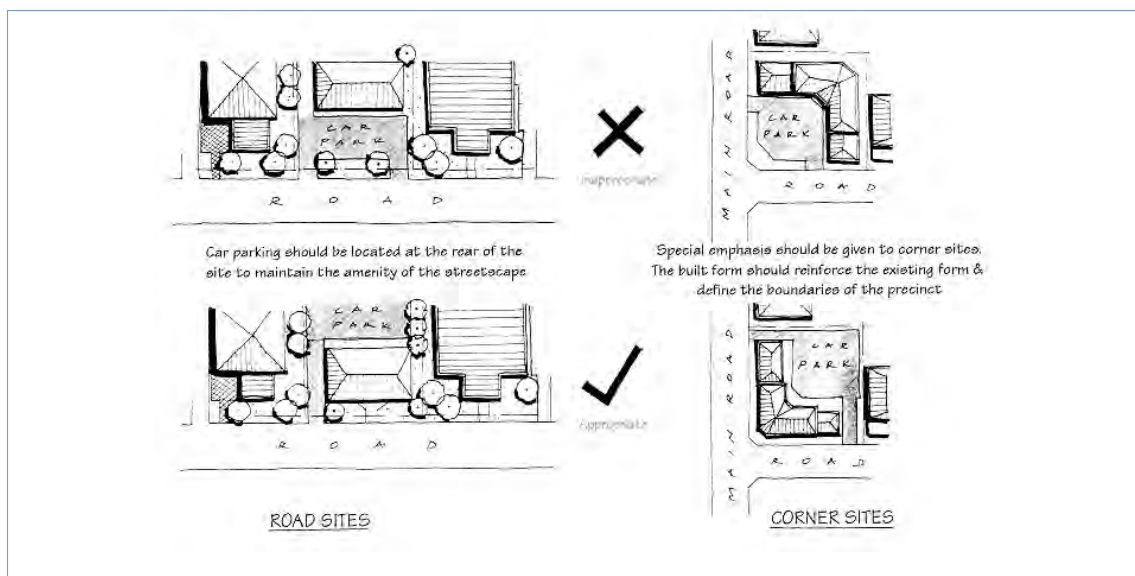


Figure 4.2(k): Car park siting principles.(I)

4.4.12 Vehicle Access and Parking

Desired Outcome

- a. Development that provides sufficient and convenient parking for residents and visitors with vehicular access that is simple, safe and direct.

Prescriptive Measures

Vehicular Access

- a. Access to garages and storage areas should be confined to side and rear facades, with access from main roads avoided.
- b. Vehicle access should be consistent with the servicing strategy depicted in the Key Development Principles diagram.

Parking

- c. Resident and visitor parking should be provided within basements.
- d. Street level parking for shoppers should be provided in convenient proximity to primary retail frontages.
- e. Any undercroft car parking should be screened and should not be located in a facade that faces a primary or secondary street frontage.
- f. Parking for service and delivery vehicles should be integrated with the design of driveways and surrounding landscaped verges, and should not visually dominate any street frontage.
- g. All ramps are to be designed as two way ramps in accordance with AS 2890.1 and AS 2890.2
- h. All ramps are to be designed in accordance with the exits and entry widths of AS 2890.1 and AS 2890.2

Ancillary Fixtures and Facilities

- i. Separate dedicated and secure storage areas for each dwelling should be provided in basement car parks suitable to accommodate larger items such as sporting equipment.

Note:

Refer to Part 1 'General' of the DCP for car parking and bicycle parking rates and ancillary general design requirements.

Main roads

Development adjoining roads that are subject to Section 2.118(2a) 2.119 of the Transport and Infrastructure SEPP require separate approval from the RMS for access to State and Regional Roads as classified by the Roads and Maritime Services (RMS). A list of classified and unclassified main roads for Hornsby Shire as of September 2016 is provided in Annexure C.

4.6.12 Vehicle Access and Parking

Desired Outcome

- a. Development that provides sufficient and convenient parking for residents and visitors with vehicular access that is simple, safe and direct.

Prescriptive Measures

Vehicular Access

- a. Access to garages and storage areas should be confined to side and rear facades, with access from main roads avoided.
- b. Vehicle access should be consistent with the servicing strategy depicted in the Key Development Principles diagram.

Parking

- c. Resident and visitor parking should be provided within basements.
- d. All ramps are to be designed as two way ramps in accordance with AS 2890.1 and AS 2890.2
- e. All ramps are to be designed in accordance with the exits and entry widths of AS 2890.1 and AS 2890.2
- f. Street level parking for shoppers should be provided in convenient proximity to primary retail frontages.
- g. Any undercroft car parking should be screened and should not be located in a facade that faces a primary or secondary street frontage.
- h. Parking for service and delivery vehicles should be integrated with the design of driveways and surrounding landscaped verges, and should not visually dominate any street frontage.

Ancillary Fixtures and Facilities

- i. Separate dedicated and secure storage areas for each dwelling should be provided in basement car parks suitable to accommodate larger items such as sporting equipment.

Public Domain

- j. Car parking areas at ground level should be screened by active uses from the street.
- k. Basement parking areas and structures should not protrude above the level of the adjacent street or public domain. Where they are visible, basement structures and vent grills should be integrated into the building and

landscape design. Ventilation grills are to block views into basement areas and, in inappropriate locations, be screened by landscaping in garden beds with a minimum soil depth of 1m.

Note:

Refer to Part 1 'General' of the DCP for car parking and bicycle parking rates and ancillary general design requirements.

Main roads

Development adjoining roads that are subject to Section 2.118(2a) 2.119 of the Transport and Infrastructure SEPP require separate approval from the RMS for access to State and Regional Roads as classified by the Roads and Maritime Services (RMS). A list of classified and unclassified main roads for Hornsby Shire as of September 2016 is provided in Annexure C.

5.1.6 Vehicle Access and Parking

Desired Outcomes

- a. Development that provides sufficient and convenient parking for residents and visitors with vehicular access that is simple, safe and direct.
- b. Developments that incorporate on-site service areas that provide for a range of industrial uses.

Prescriptive Measures

Vehicular Access

- a. Direct vehicular access to main roads should be avoided where alternative access is available via service lanes or local roads.
- b. For development in the Dural Service Centre, vehicular access to New Line Road should be via service lanes and vehicular access to Old Northern Road should be via service roads, in accordance with the Traffic Management Strategy ((see Figures 5.1(d) and 5.1(e)).

Parking

- c. Parking should be provided to the rear of buildings or below ground level.
- d. Parking may be considered in front setback areas where site constraints warrant. A maximum of 50% of the required front setback area should be used for carparking and driveway areas.
- e. Car parking should be screened from the street by landscaping.

Service Vehicles

- f. Each industrial unit/ premises should have access to a loading and unloading area on-site.
- g. Where a development consists of multiple industrial units, at least 1 communal loading area that is capable of accommodating an articulated vehicle should be provided on-site.
- h. Loading areas should have minimum dimensions of 3 metres x 7 metres and have turning areas that comply with AS 2890.2, applicable to the size of vehicle that may service the site.

Note:

Refer to Part 1 'General' of the DCP for car parking, service vehicle, bicycle parking provisions and ancillary general design requirements.

5.1.7 Traffic Management Work

Desired Outcomes

- a. Traffic management works that provide for the safe and efficient movement of vehicles to, from and within precincts.

Prescriptive Measures

- a. Applicants should liaise with Roads and Maritime Services and Council to determine the extent of any road works required along New Line Road, in accordance with the Traffic Management Strategy.
- b. Service lanes should be provided in accordance with the Traffic Management Strategy (see Figures 5.1(d) and 5.1(e)).

Main roads

Development adjoining roads that are subject to Section 2.118(2a) 2.119 of the Transport and Infrastructure SEPP require separate approval from the RMS for access to State and Regional Roads as classified by the Roads and Maritime Services (RMS). A list of classified and unclassified main roads for Hornsby Shire as of September 2016 is provided in Annexure C.

6.2 Urban Subdivision

6.2.1 Residential Lands Subdivision within Hornsby LGA

The following provides controls for subdivision in the R2 Low Density Residential Zone.

Desired Outcomes

- Subdivision design should maintain appropriately shaped lots to accommodate a dwelling and associated development that is compatible with a low density residential environment.
- Subdivision design should provide setbacks to developable areas that will:
 - complement the streetscape,
 - provide for landscaping,
 - protect landscape features, and
 - provide separation between existing and future dwellings.

Prescriptive Measures

Lot Size

- The minimum lot size is depicted on the Minimum Lot Size map, as summarised in the following:

Table 6.2(a): Minimum Lot Size - R2 Zone

HLEP Area	Minimum Lot Size
I	500m ²
M	600m ²

- In calculating the area of a lot resulting from a subdivision of land, the area of any accessway, right of carriageway or the like is to be excluded.
- The size of the proposed lot may need to be greater than the area prescribed in the table above in order to achieve the minimum setbacks required from significant landscape features or to address site constraints.

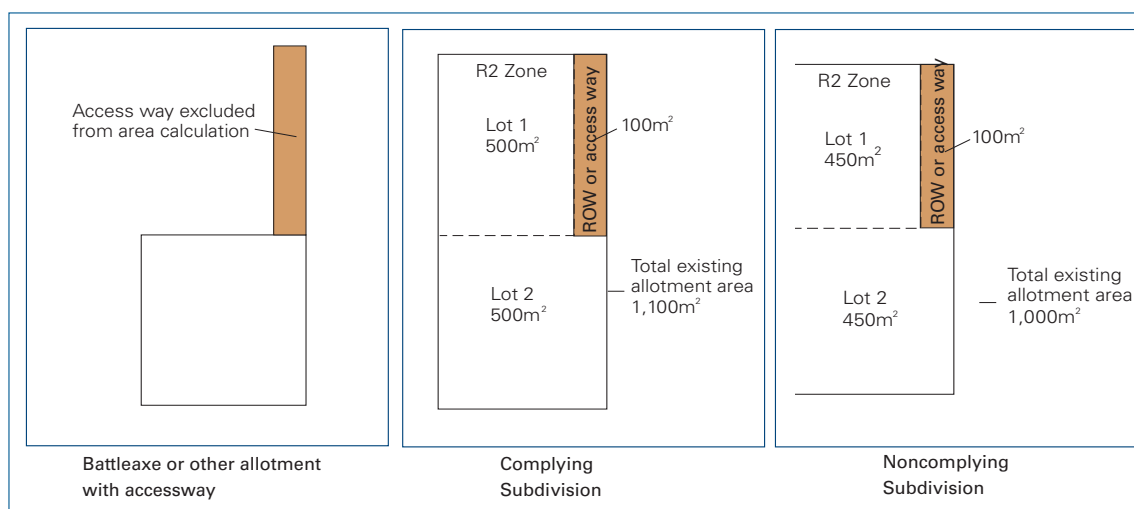


Figure 6.2(a) - Illustration of lot size controls in the R2 zone, within area I on the HLEP Lot Size Map.(I)

Lot Shape

- d. Lot design should maintain a minimum lot width that is compatible with the subdivision pattern, ~~measured at the building line adjacent to the primary street frontage,~~ as detailed in Table 6.2(b). ~~In accordance with Figure 6.2(b), lot width is measured at:~~

- n The building line adjacent to the primary street frontage, or
- n across the front of a building envelope within battle-axe allotments.

Table 6.2(b): Minimum Lot Width - R2 Zone

HLEP Area	Minimum Lot Size	Minimum Lot width
I	500m ²	12m
M	600m ²	15m

- e. Lots should be designed to allow the construction of a building, principal private open space area and carriageway with a maximum cut and fill of 1 metre from natural ground level.

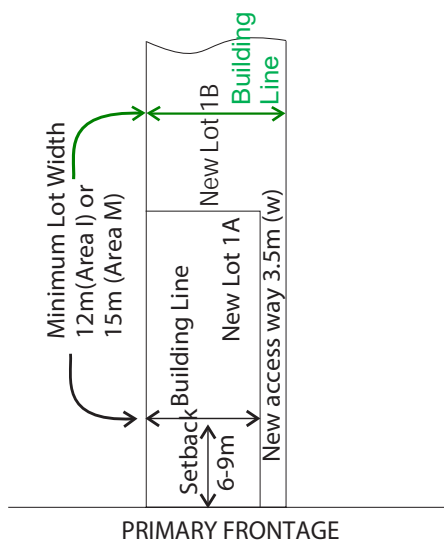


Figure 6.2(b):
Minimum lot width required ~~at the primary street frontage boundary~~ for all allotments.(I)

- f. Lot design should identify a potential developable area, as detailed in Figure 6.2(c).

This area is to accommodate the following:

- a building envelope of 200m² with a minimum dimension of 10 metres;
- a principal private open space area,
- area for parking 2 cars behind the building line, and
- comply with the general provisions in Section 6.1

- g. If an existing dwelling is to be retained, the proposed lot should be of sufficient size and design so that the dwelling complies with the 'Dwelling House' element in Section 3.1 of this DCP.

Note:

A building envelope is the area of land identified for the purpose of the future erection of a dwelling and its immediate curtilage.

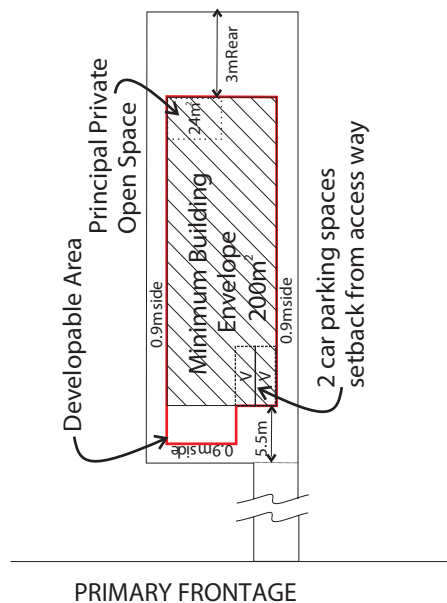


Figure 6.2(c):
Proposed subdivision plans should identify a potential developable area for each new lot.(I)

Setbacks

- h. Setbacks to the proposed building envelope and ancillary structures should comply with Table 6.2(c).

Table 6.2(c): Minimum Boundary Setbacks

Setback	Minimum Building Envelope Setback
Front boundary (primary frontage)	6m to local roads and 9m to designated roads, except for the following: <ul style="list-style-type: none"> on local roads, where an existing setback of 7.6m or greater exists, it may be necessary to conform to this setback to maintain the streetscape character, and 3m to Brooklyn Road, Brooklyn, and 9m to roads in Cherrybrook
Waterfront Setback	See Clause 6.1 of HLEP Foreshore Building Line Map
Secondary boundary (corner lots)	3m
Setbacks from internal accessways	5.5m to a garage/ carport
Side boundary	0.9m
Rear boundary	5.3m

- i. For a site that:

- Adjoins parallel roads, the front boundary setback control applies to both the primary frontage and the parallel road boundary.
- Is a battle-axe lot, the setback on the opposite side of the lot to the rear setback, is taken to be a side setback (refer to Figure 6.2 (d)).

- j. The setback of the building envelope and ancillary structures from the property boundary may need to be increased to comply with the general provisions in Part 1 and Section 6.1 of this DCP.

Note:

Designated roads

Designated roads are Council identified roads that require development to have an increased setback from the road edge, consistent with the established streetscape. A list of designated roads is provided in Annexure C.

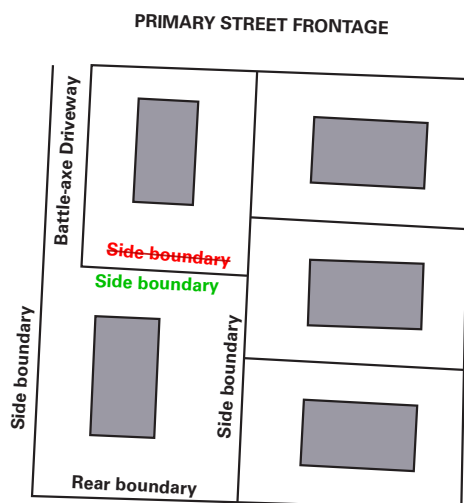


Figure 6.2 (d): Setbacks on battle-axe lots.(l)

Open Space and Landscaping

- a. Subdivision design should provide a principal private open space area of 24m² for each lot. This area is to be generally level, with a minimum width of 3 metres, sited adjacent to the building envelope and behind the front setback.
- b. Subdivision design should demonstrate that the minimum landscaped area on a property complies with Table 6.2(d):

Table 6.2(d): Minimum Landscaped Area

Lot size	Minimum landscaped area (% of the lot size)
Up to 599m ²	20%
600m ² to 899m ²	30%
900m ² to 1499m ²	40%
1500m ² or larger	45%

- c. Areas included as part of the minimum landscaped area should have a minimum width of 1.5 metres.
- d. At least 50 percent of the minimum landscaped area should be located behind the building line to the primary road frontage.
- e. A proportion of the front yard should be maintained as landscaped area as follows:
 - 25 percent of the front yard for lots less than 18 metres wide, and
 - 50 percent of the front yard for lots greater than 18 metres wide.

Note:

Landscaped area refers to a permeable area capable of growing plants, grasses and trees. It does not include the 200m² building envelope, the principle private open space area, clothes drying areas, driveways, and other structures or hard paved areas.

Lot size (or site area) in relation to development, means the area of the lot to which an application for consent to carry out the development relates, excluding:

- (a) any land on which the development is not permitted under an environmental planning instrument, and
- (b) if a lot is a battle-axe or other lot with an access handle, the minimum lot size excludes the area of the access handle.

6.2 Urban Subdivision

6.2.1 Residential Lands Subdivision City of Parramatta @ 5

The following provides controls for subdivision in the R2 Low Density Residential Zone.

Desired Outcomes

- Subdivision design should maintain appropriately shaped lots to accommodate a dwelling and associated development that is compatible with a low density residential environment.
- Subdivision design should provide setbacks to developable areas that will:
 - complement the streetscape,
 - provide for landscaping,
 - protect landscape features, and
 - provide separation between existing and future dwellings.

Prescriptive Measures

Lot Size

- The minimum lot size is depicted on the Minimum Lot Size map, as summarised in the following:

Table 6.2(a): Minimum Lot Size - R2 Zone

HLEP Area	Minimum Lot Size
I	500m ²
M	600m ²

- In calculating the area of a lot resulting from a subdivision of land, the area of any accessway, right of carriageway or the like is to be excluded.
- The size of the proposed lot may need to be greater than the area prescribed in the table above in order to achieve the minimum setbacks required from significant landscape features or to address site constraints.

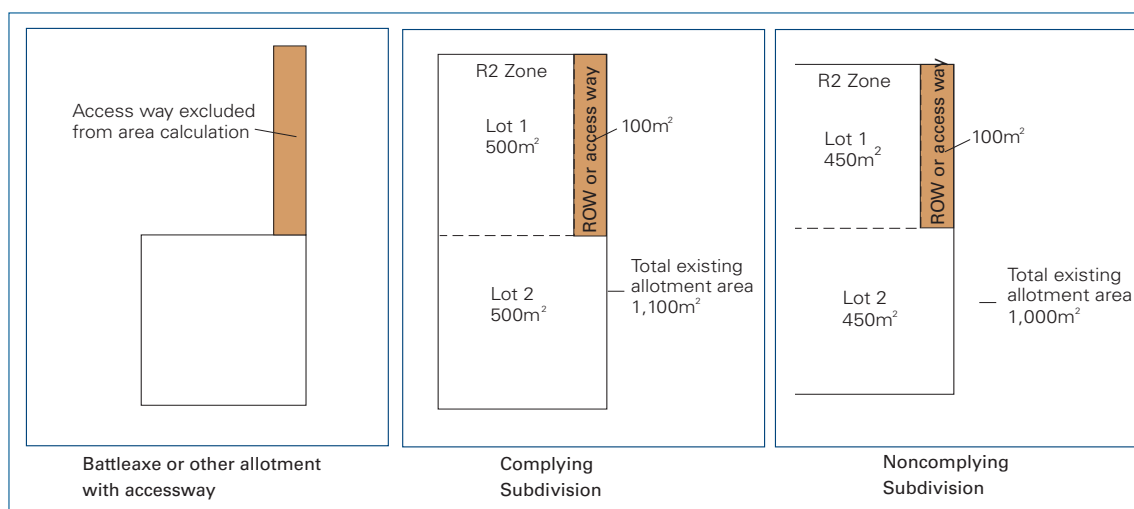


Figure 6.2(a) - Illustration of lot size controls in the R2 zone, within area I on the HLEP Lot Size Map.(I)

Lot Shape

- d. Lot design should maintain a minimum lot width that is compatible with the subdivision pattern, measured at the building line adjacent to the primary street frontage, as detailed in Table 6.2(b).

Table 6.2(b): Minimum Lot Width - R2 Zone

HLEP Area	Minimum Lot Size	Minimum Lot width
I	500m ²	12m
M	600m ²	15m

- e. Lots should be designed to allow the construction of a building, principal private open space area and carriageway with a maximum cut and fill of 1 metre from natural ground level.

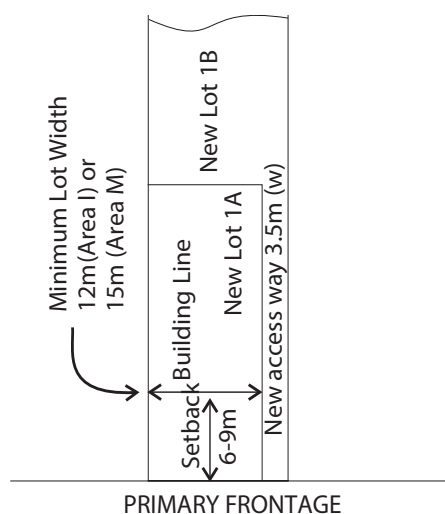


Figure 6.2(b):
Minimum lot width required at the primary street frontage boundary.(I)

- f. Lot design should identify a potential developable area. This area is to accommodate the following:

- a building envelope of 200m² with a minimum dimension of 10 metres;
- a principal private open space area,
- area for parking 2 cars behind the building line, and
- comply with the general provisions in Section 6.1

- g. If an existing dwelling is to be retained, the proposed lot should be of sufficient size and design so that the dwelling complies with the 'Dwelling House' element in Section 3.1 of this DCP.

Note:

A building envelope is the area of land identified for the purpose of the future erection of a dwelling and its immediate curtilage.

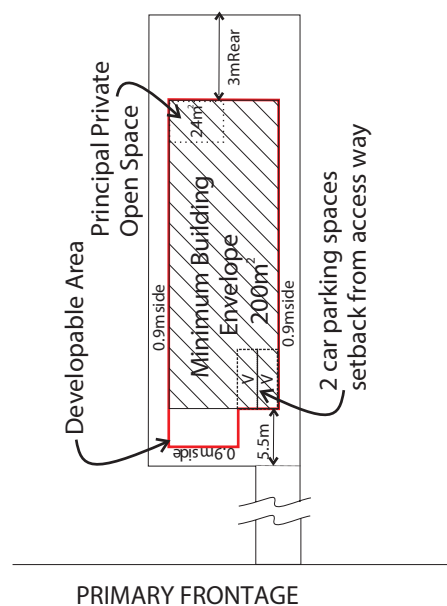


Figure 6.2(c):
Proposed subdivision plans should identify a potential developable area for each new lot.(I)

Setbacks

- h. Setbacks to the proposed building envelope and ancillary structures should comply with Table 6.2(c).

Table 6.2(c): Minimum Boundary Setbacks

Setback	Minimum Building Envelope Setback
Front boundary (primary frontage)	6m to local roads and 9m to designated roads, except for the following: <ul style="list-style-type: none"> on local roads, where an existing setback of 7.6m or greater exists, it may be necessary to conform to this setback to maintain the streetscape character, and 3m to Brooklyn Road, Brooklyn, and 9m to roads in Cherrybrook
Waterfront Setback	See Clause 6.1 of HLEP Foreshore Building Line Map
Secondary boundary (corner lots)	3m
Setbacks from internal accessways	5.5m to a garage/ carport
Side boundary	0.9m
Rear boundary	5m

- i. For a site that:

- Adjoins parallel roads, the front boundary setback control applies to both the primary frontage and the parallel road boundary.
- Is a battle-axe lot, the setback on the opposite side of the lot to the rear setback, is taken to be a side setback (refer to Figure 6.2 (d)).

- j. The setback of the building envelope and ancillary structures from the property boundary may need to be increased to comply with the general provisions in Part 1 and Section 6.1 of this DCP.

Note:

Designated roads

Designated roads are Council identified roads that require development to have an increased setback from the road edge, consistent with the established streetscape. A list of designated roads is provided in Annexure C.

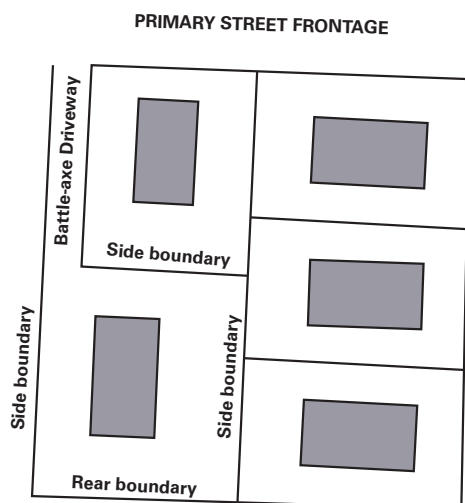


Figure 6.2 (d): Setbacks on battle-axe lots.(l)

Open Space and Landscaping

- a. Subdivision design should provide a principal private open space area of 24m² for each lot. This area is to be generally level, with a minimum width of 3 metres, sited adjacent to the building envelope and behind the front setback.
- b. Subdivision design should demonstrate that the minimum landscaped area on a property complies with Table 6.2(d):

Table 6.2(d): Minimum Landscaped Area

Lot size	Minimum landscaped area (% of the lot size)
Up to 599m ²	20%
600m ² to 899m ²	30%
900m ² to 1499m ²	40%
1500m ² or larger	45%

- c. Areas included as part of the minimum landscaped area should have a minimum width of 1.5 metres.
- d. At least 50 percent of the minimum landscaped area should be located behind the building line to the primary road frontage.
- e. A proportion of the front yard should be maintained as landscaped area as follows:
 - 25 percent of the front yard for lots less than 18 metres wide, and
 - 50 percent of the front yard for lots greater than 18 metres wide.

Note:

Landscaped area refers to a permeable area capable of growing plants, grasses and trees. It does not include the 200m² building envelope, the principle private open space area, clothes drying areas, driveways, and other structures or hard paved areas.

Lot size (or site area) in relation to development, means the area of the lot to which an application for consent to carry out the development relates, excluding:

- (a) any land on which the development is not permitted under an environmental planning instrument, and
- (b) if a lot is a battle-axe or other lot with an access handle, the minimum lot size excludes the area of the access handle.

6.4 Accessway Design

6.4.1 Residential and Rural Lands Accessway Design

The following provides controls for the accessway design of subdivisions in the low density residential areas and rural areas of Hornsby Shire. For other localities refer to the applicable parts of the DCP. For example, for subdivision in an Industrial Area the provisions of 5.1 Industrial land will apply.

Desired Outcomes

- To ensure access along private accessways to all new lots is simple, safe and direct.
- Driveways should not be visually intrusive to the existing streetscape.
- To limit the number of driveway crossings and additional dwellings with direct access to main roads to limit the cumulative impacts on traffic flows and safety.

Prescriptive Measures

Location

- Accessways should connect to local roads. No new direct vehicle access should be provided to the following main roads:-
 - ¾ Castle Hill Road (between Old Northern Road and Edward Bennett Drive, Cherrybrook)
 - ¾ Old Northern Road (between Castle Hill Road and New Line Road, Cherrybrook)
 - ¾ New Line Road (between New Farm Road and Sebastian Drive, West Pennant Hills, Cherrybrook and Dural)
 - ¾ Boundary Road (between New Line Road and Cherrybrook Road, Cherrybrook)
- Additional vehicle crossings should be limited to all other main roads .
- The distance between adjacent driveways should be less than 3 metres or more than 6 metres, to avoid the creation of an undersized on street parking space.
- On the eastern side of Arcadia Road between Galston Road and Gribbenmount Road:
 - ¾ vehicular crossings and driveways should be consolidated where possible, and
 - ¾ street tree planting should be provided within the road reserve.

Notes:

Refer to AS 2890.1:2004 for sight distance at driveway access exits.

Refer to Section 1.4.1. of the DCP for general design requirements related to transport and parking.

General Design

- The dimensions of an accessway should comply with Table 6.4(a).

Table 6.4(a): Accessways to Low Density Residential and Rural Lots

Lots and/or dwellings	Accessway width (min)	Carriageway width (min)	Landscape verge (min total)
1-3	3.5m	3.0m including kerbs	0.5m
4-6	4m	3.0m including kerbs	1m
7-24	6.65m	5.65m including kerbs	1m
>24	Comply with Council's H.S.C. Civil Works Design and Construction Specification		

- Carriageways should have a maximum grade of 25% at any point with a maximum average grade of 20% over the length of the carriageway for subdivisions of 1 to 3 lots. For subdivisions of 4 or more lots, the maximum gradient is 20%.

Note:

The carriageway is an unencumbered pavement with no building encroachments (including eaves) with a minimum height clearance of 4.5 metres.

Common Turning Areas

- g. Accessways serving 2 or more lots should incorporate a common turning area, designed to allow the 85% Design Car Turning Path in accordance with AS 2890.1 and AS 2890.2, where:

- ¾ the site has a slope greater than 15%,
- ¾ the accessway fronts a main road or highly pedestrianised area, or
- ¾ where vehicles would otherwise have to reverse more than 50 metres.

Note:

Main roads

Development adjoining roads that are subject to Section ~~2.118(2a)~~ 2.119 of the Transport and Infrastructure SEPP require separate approval from the RMS for access to State and Regional Roads as classified by the Roads and Maritime Services (RMS). A list of classified and unclassified main roads for Hornsby Shire as of September 2016 is provided in Annexure C.

A **highly pedestrianised area** includes sites located in close proximity to schools, shopping centres, bus stops, places of worship and other busy community facilities.

Passing Bays

- h. A passing bay with a minimum width of 5.5 metres and depth of 6 metres and transition of 4 metres should be provided adjacent to the street boundary where the accessway:

- ¾ serves 7 or more lots, or
- ¾ serves 2 or more lots and fronts a main road.

- i. A passing bay with a minimum width of 5.5 metres should be provided every 40 metres where a long common driveway is proposed.

Waste Collection Vehicles

- j. Waste collection vehicles should be accommodated on-site if the accessway serves 7 or more lots, or where site constraints require.

- k. When an on-site waste collection area is required, the development should:

- ¾ identify a bin collection area inside the property,
- ¾ enable waste collection vehicles to enter and exit the site in a forward direction, and
- ¾ be designed to accommodate Council's large waste collection vehicle per Section 1C.2.3 of the DCP.

Note:

The requirement to accommodate a large waste collection vehicle may result in the maximum carriageway gradient of 20-25% as prescribed in the general design controls, being unachievable.

Pedestrian and Bicycle Links

- l. The subdivision design should provide convenient, obvious and safe pedestrian and bicycle links from the site to public transport facilities and local facilities.

Street Lighting

- m. Accessways serving 7 or more properties should provide street lighting per AS 1158.3 and AS 4282.

Note:

Compliance with AS 1158.3 and AS 4282 may require bollard style lighting along private accessways to provide for lighting whilst limiting light spill into residential dwellings.

9.6.14 Public Domain and Traffic Management Works

Desired Outcomes

- a. A public domain that encourages vitality around and within development precincts.
- b. Traffic management works which provide for the safe and efficient movement of vehicles to, from and within precincts.

Prescriptive Measures

Public Domain

- a. Development of the public domain should make each precinct an attractive place that encourages development and provides amenity for residents.
- b. Embellishment of the public domain should include street furniture, new street plantings, and footpath improvements.
- c. All active street frontages in mixed use developments should have fully paved verges.
- d. Pedestrian linkages shown on the key development principles diagrams and town centre linkage diagrams (Annexure B) should be provided and reinforced as safe, accessible and vibrant pedestrian areas.
- e. Mixed use development within centres should enhance the role of the public domain as a meeting and gathering place and should encourage active use of the public domain through active street frontages.
- f. Where required, ground level walkways between mixed use buildings should be open air, attractive pedestrian thoroughfares which encourage activity.
- g. Balconies should not be located on, or overhang the road reservation.
- h. For development incorporating shopfront awnings, the awnings should be continuous and setback from the edge of the kerb in accordance with Council or the Roads and Maritime Services requirements.

Note:

Main roads

Development adjoining roads that are subject to Section 2.418(2a) 2.119 of the Transport and Infrastructure SEPP require separate approval from the RMS for access to State and Regional Roads as classified by the Roads and Maritime Services (RMS). A list of classified and unclassified main roads for Hornsby Shire as of September 2016 is provided in Annexure C.

Outdoor Dining

- i. Outdoor dining areas should be located in areas with good amenity, landscape, outlook, solar access in winter, shading in summer and a compatible local traffic environment.

Note:

Outdoor dining proposed on Council land should comply with Council's Outdoor Dining Code.

Traffic Management Works

- j. Traffic Management works should be undertaken in accordance with the traffic improvements identified in the key development principles diagrams.
- k. Council or the relevant authority will undertake the necessary traffic management improvements located on public land and roads. Development should be designed to accommodate and complement the proposed traffic improvements or offer alternative traffic management solutions.
- l. Development proposing alternative traffic management solutions should be accompanied by a comprehensive traffic assessment.

Note:

This DCP will inform Council's Civic Works Program and Street Tree Planting Program.